



ADDENDUM NO. 1

This addendum consists of 16 pages

TO: All Bid Packet Holders

FROM: City of Kenai Public Works Department

DATE: May 22, 2023

SUBJECT: Invitation to Bid – 2023 N. Willow St. Roadway Improvements

DUE DATE: ***June 1, 2023, by no later than 10:00 AM***

Bidders must acknowledge receipt of this Addendum in the appropriate place on the Bid Form. Failure to do so may result in the disqualification or rejection of the bid.

Note: Information in this addendum takes precedence over original information. All other provisions of the document remain unchanged.

01-01 Clarification

Please see the attached revised Construction Drawings with minor additions to scope.
Please see the attached Bid Schedule detailing the requested quantities of work to be included with Bid.
Please see the attached new specification Section 20.32 – Compaction Control by the Contractor.

End of Addendum 1

Bid Schedule - 2023 N Willow Street Improvements - Addendum 1

BID SCHEDULE - BASE BID					
Item Number	Pay Item Description	Unit	Estimated Quantity	Unit Bid Price	Amount Bid
20.02-A	Storm Water Pollution Prevention Plan	Lump Sum	1	(LUMP SUM)	
20.02-B	SWPPP-Implementation	Conting. Sum	1	\$ 5,000.00	\$ 5,000.00
20.04	Clearing and Grubbing	Lump Sum	1	(LUMP SUM)	
20.09	Remove Pavement	Square Yard	13,805		
20.10a	Unusable Excavation	Cubic Yard	4,000		
20.11	Grading Existing Surfaces	Linear Foot	3,610		
20.21a	Classified Fill and Backfill (Type IIA)	Ton	4,800		
20.21b	Classified Fill and Backfill (Type III)	Ton	4,000		
20.22	Leveling Course (2" Roadway, 4" Shoulders)	Ton	1,520		
20.28	Reconstruction Driveway (Paved)	Each	19		
20.31	Drainage Gallery	LF	300		
20.32	Compaction Control by the Contractor	Lump Sum	1	(LUMP SUM)	
40.06	Asphalt Concrete Pavement (Class E, 2" Thickness)	Ton	1,650		
50.06a	Adjust SS Manhole Cover & Frame to Finish Grade	Each	4		
55.20a	Culvert (15-Inch, CMP or CPEP, 16 Ga., Round)	Linear Foot	830		
55.20b	Culvert (18-Inch, CMP or CPEP, 16 Ga., Round)	Linear Foot	162		
55.20c	Culvert End Section (15-Inch)	Each	36		
55.20d	Culvert End Section (18-Inch)	Each	8		
60.03	Adjust Valve Box to Finish Grade	Each	3		
65.02	Construction Survey Measurement	Lump Sum	1	(LUMP SUM)	
70.07	Remove Pipe (Existing Culverts)	Linear Foot	425		
70.10a	Traffic Markings (Solid Yellow, 4")	Linear Foot	4800		
70.10b	Traffic Markings (Solid White, 4")	Linear Foot	7220		
70.10c	Traffic Markings (Solid White Stop Bar, 24")	Linear Foot	315		
70.12	Traffic Maintenance	Lump Sum	1	(LUMP SUM)	
70.22	Adjust Monument to Finish Grade	Each	1		
75.03	Topsoil (4" Depth)	MSF	38.1		
75.04	Seeding (Schedule D)	MSF	38.1		
90.10	Force Account Work	Conting. Sum	1	\$ 50,000.00	\$ 50,000.00
90.11	Existing Utilities In Construction Zone	Lump Sum	1	(LUMP SUM)	
90.13	Mobilization and Demobilization	Lump Sum	1	(LUMP SUM)	
Total Base Bid Estimate					

ADD THE FOLLOWING SECTION:

SECTION 20.32 – COMPACTION CONTROL BY THE CONTRACTOR

Article 32.1 Description

The Contractor shall provide field compaction testing for quality control at each of the project sites and based upon the following schedule:

- One (1) compaction test per 600 lineal feet per lift of classified fill, leveling course, and asphalt pavement.

The maximum density shall be based upon ASTM-D-1557, Modified Proctor Density. The generation of the maximum density curves and the field density testing shall be certified by an engineer registered in the State of Alaska. Copies of all proctor curves and field density test results shall be made available immediately to the City and the Engineer.

If a field density test does not meet the contract compaction requirements, the Contractor shall initiate additional compaction efforts. Additional lifts of earthwork shall not be placed over a zone that has failed a field density test until a passing test has been achieved. All re-testing of failed areas shall be performed by the Contractor at no additional cost to the City.

The Contractor shall notify the Engineer 24 hours prior to performing any field density tests. The Owner reserves the right to observe any or all field density testing performed by the Contractor. The Owner also reserves the right to perform independent field density testing to confirm the results of the Contractor's quality control program.

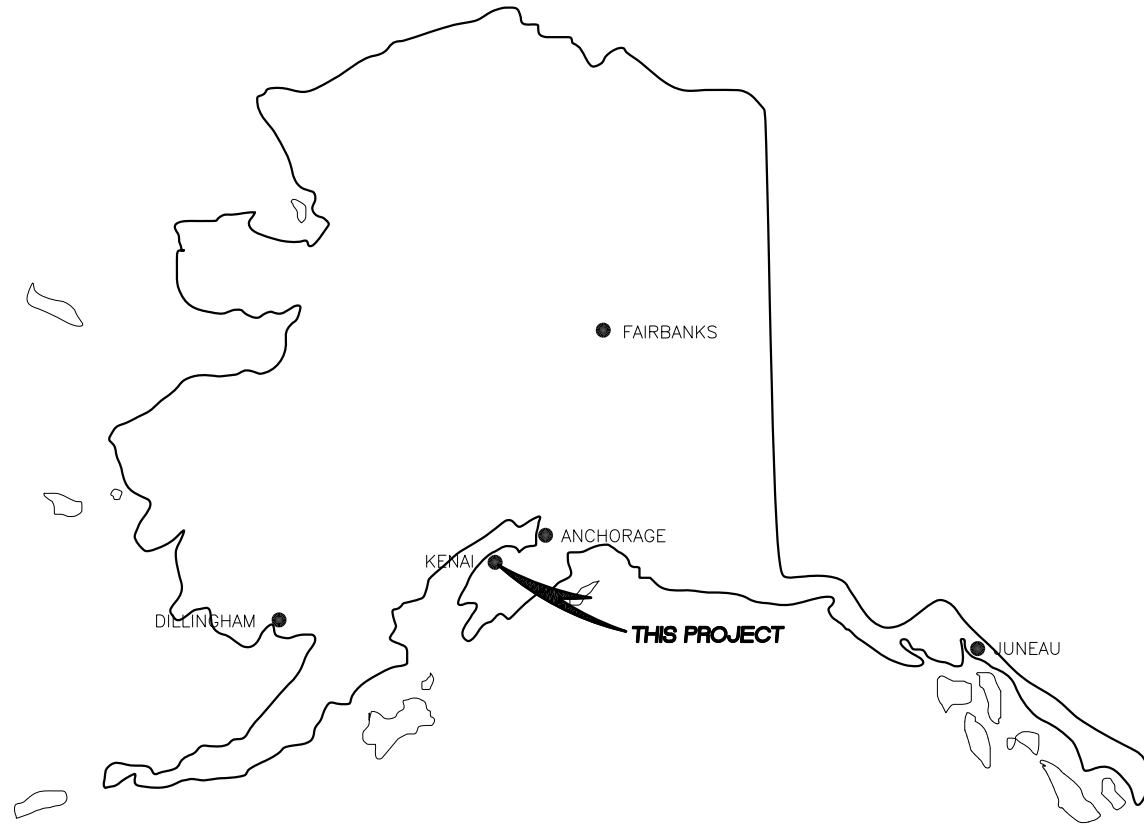
Article 32.2 Measurement

No measurement will be made for this item of work.

Article 32.3 Basis of Payment

Payment will be made under the following:

ITEM	UNIT
Compaction Control by the Contractor	Lump Sum



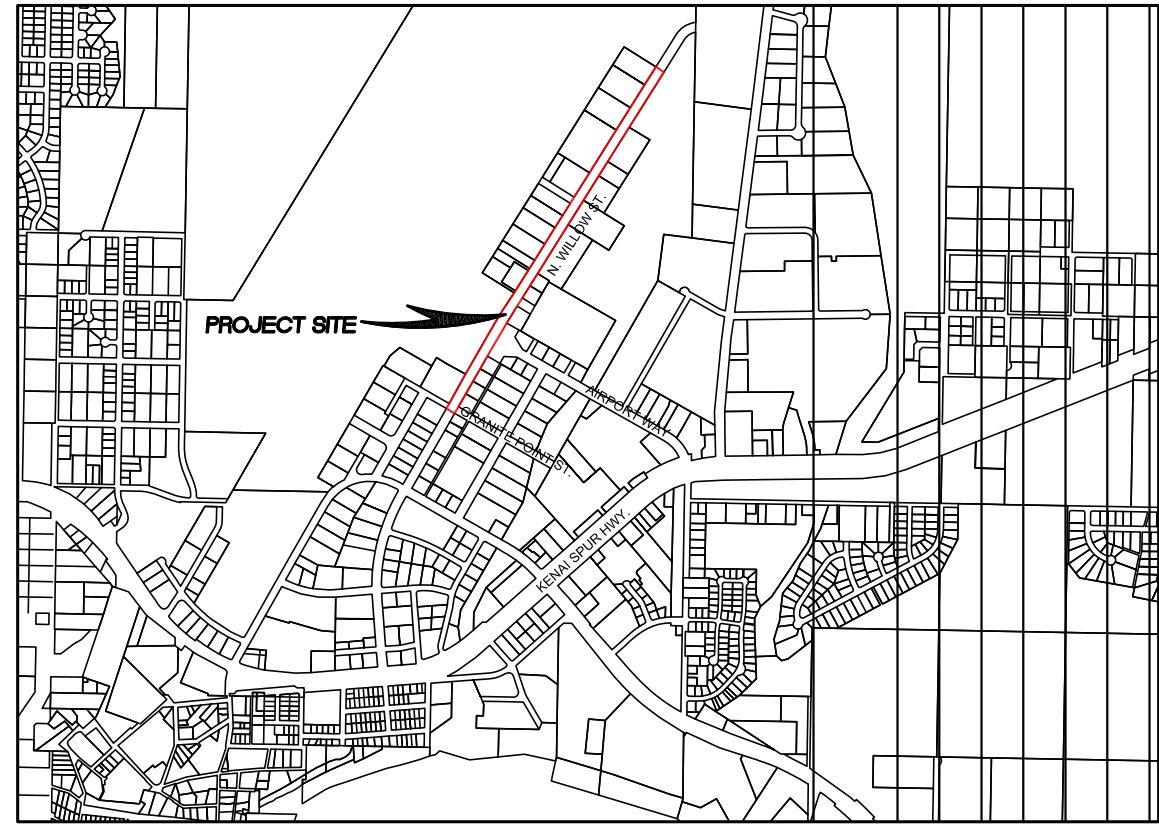
2023 N. WILLOW STREET ROADWAY IMPROVEMENTS FOR BID

CITY OF KENAI

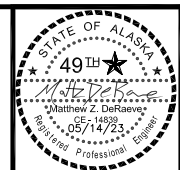


DRAWING INDEX

DRAWING	SHEET
TITLE SHEET AND LOCATION MAP	C1
LEGEND, ABBREVIATIONS, AND GENERAL NOTES	C2
N WILLOW ST - PLAN AND PROFILE STA. 0+00 TO 6+50	C3
N WILLOW ST - PLAN AND PROFILE STA. 6+50 TO 13+00	C4
N WILLOW ST - PLAN AND PROFILE STA. 13+00 TO 19+50	C5
N WILLOW ST - PLAN AND PROFILE STA. 19+50 TO 26+00	C6
N WILLOW ST - PLAN AND PROFILE STA. 26+00 TO 32+50	C7
N WILLOW ST - PLAN AND PROFILE STA. 32+50 TO 37+00	C8
TYPICAL ROAD SECTIONS	C9
TYPICAL ROAD SECTIONS	C10
TYPICAL DETAILS	C11
TYPICAL DETAILS	C12
BOREHOLE LOGS	C13

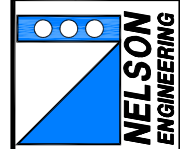


LOCATION MAP
SCALE: NOT TO SCALE
NORTH



NO.	REVISION	DATE
1	REV. - ADDENDUM 1	5/22/23

CONSULTING ENGINEERS
STRUCTURAL/CIVIL
155 BIDARCA ST
KENAI, AK 99611
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NELSONENGINEER@ALASKA.NET



2023 N. WILLOW ST. ROADWAY IMPROVEMENTS
CITY OF KENAI
KENAI, ALASKA
TITLE SHEET AND LOCATION MAP

PROJECT NO.
2022030.2
DRAWN BY:
GTP/MZD
CHECKED BY:
MJD/MZD
DATE: 5/14/2023
SCALES: NOTED
HORIZ. NOTED
VERT. NOTED
SHEET: C1
1 OF 13

LEGEND

EXISTING (E)	PROPOSED (P)	
		CENTERLINE ROAD
		CONTOUR LINES
		CUT/FILL DAYLIGHT
		EDGE OF PAVEMENT
		EDGE OF R.O.W.
		FLOW DIRECTION
		TREELINE
		UTILITY - COMMUNICATION
		UTILITY - ELECTRIC (OVERHEAD)
		UTILITY - ELECTRIC (UNDERGROUND)
		UTILITY - GAS
		UTILITY - WATER
		UTILITY - SEWER
		COMMUNICATION PEDESTAL
		CONTROL POINT
		CULVERT
		ELECTRICAL TRANSFORMER
		HYDRANT
		POWER POLE
		SANITARY SEWER MANHOLE
		TEST HOLE LOCATION
		VALVE BOX
		ASPHALT PAVEMENT

ABBREVIATIONS

AC	ASPHALT CONCRETE, ACRE	LT	LEFT
ADA	AMERICAN DISABILITIES ACT	LVC	LENGTH OF VERTICAL CURVE
ALUM	ALUMINUM	MAX	MAXIMUM
APPROX	APPROXIMATE	ME	MATCH EXISTING
AVG	AVERAGE	MIN	MINIMUM
BH	BOREHOLE	MKR	MARKER POST
BLDG	BUILDING	MON	MONUMENT
BOP	BOTTOM OF PIPE	N	NORTH
BVCE	BEGIN VERTICAL CURVE	NFS	NON FROST SUSCEPTIBLE
	ELEVATION	NTS	NOT TO SCALE
BVCS	BEGIN VERTICAL CURVE	OC	ON CENTER
	STATION	OFF	OFFSET
C.O.	CLEANOUT	OHE	OVERHEAD ELECTRIC
CIP	CAST IRON PIPE	PI	POINT OF INTERSECTION
CL	CENTER LINE, CLASS	PL	PROPERTY LINE
CMP	CORRUGATED METAL PIPE	PP	POWER POLE
CNTR	CENTER	PT	POINT
CONC	CONCRETE	PVI	POINT OF VERTICAL
CP	CONTROL POINT		INTERSECTION
DIA	DIAMETER	PVMT	PAVEMENT
DW	DRIVEWAY	R	RADIUS, RECORD
DWG	DRAWING	REQ'D	REQUIRED
E	EAST, ELECTRIC	ROW	RIGHT-OF-WAY
EA	EACH	S	SOUTH
EG	EXISTING GROUND	S/W	SIDEWALK
EL,ELEV	ELEVATION	SW	SWALE
EOP	END OF PROJECT	SH	SHOULDER
EP	EDGE OF PAVEMENT	SS	SANITARY SEWER
ESMT	EASEMENT	SSMH	SANITARY SEWER MANHOLE
EXIST	EXISTING	STA	STATION
FF	FINISH FLOOR	TBC	TOP BACK OF CURB
FG	FINISH GRADE	TBM	TEMPORARY BENCHMARK
FT	FOOT, FEET	TC	TOP OF CONCRETE
F&I	FURNISH AND INSTALL	TYP	TYPICAL
GD	GROUND	UGC	UNDERGROUND COMMUNICATION
GR	GRADE	UGE	UNDERGROUND ELECTRIC
HYD	HYDRANT	VB	VALVE BOX
INV	INVERT	W	WATER, WEST
L	LENGTH	WS	WATER SERVICE
LF	LINEAR FEET		

GENERAL NOTES

- DRAWINGS BASED ON OF TOPOGRAPHIC SURVEY BY PENINSULA SURVEYING, LLC, DATED AUGUST 18, 2022.
- LOCATION OF UNDERGROUND UTILITIES ARE APPROXIMATE. ACTUAL DEPTH, NUMBER AND LOCATION UNKNOWN. BURIED UTILITIES OTHER THAN THOSE SHOWN ON THE PLANS MAY BE PRESENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATION, IDENTIFYING, AND WORKING AROUND ALL UTILITIES WITHIN THE PROJECT LIMITS AT NO ADDITIONAL COST TO THE OWNER. CALL FOR LOCATES PRIOR TO EXCAVATION, ALASKA DIGLINE 1-800-478-3121. THE CITY OF KENAI DOES NOT PARTICIPATE IN THE ALASKA DIGLINE. CONTRACTOR SHALL CONTACT CITY OF KENAI PUBLIC WORKS AT 907-283-8236 FOR WATER/SEWER/STREET LIGHT UTILITY LOCATES.
- THE FOLLOWING ALASKA STATUTES APPLY TO WORK NEAR OVERHEAD ELECTRIC LINES:
AS 18.60.670 PROHIBITION AGAINST PLACEMENT OF EQUIPMENT NEAR ELECTRICAL LINE AND CONDUCTORS.

(1) PLACE ANY TYPE OF TOOL, EQUIPMENT, MACHINERY, OR MATERIAL THAT IS CAPABLE OF LATERAL, VERTICAL, OR SWINGING MOTION, WITHIN 10 FEET OF A HIGH VOLTAGE OVERHEAD ELECTRICAL LINE OR CONDUCTOR.

(2) STORE, OPERATE, ERECT, MAINTAIN, MOVE, OR TRANSPORT TOOLS, MACHINERY, EQUIPMENT, SUPPLIES, MATERIALS, APPARATUS, BUILDINGS, OR OTHER STRUCTURES WITHIN 10 FEET OF A HIGH VOLTAGE OVERHEAD ELECTRICAL LINE OR CONDUCTOR.

AS 18.60.680 ANY WORK WITHIN MINIMUM DISTANCE STATED ABOVE SHALL REQUIRE CONTACT WITH HEA TO INSTALL TEMPORARY MECHANICAL BARRIERS, TEMPORARY DE-ENERGIZATION AND GROUNDING, OR TEMPORARY RAISING OF CONDUCTORS.
- IF NOT DETAILED IN THESE DRAWINGS AND SPECIFICATIONS, ALL APPLICABLE CONSTRUCTION SHALL BE BUILT IN ACCORDANCE WITH MUNICIPALITY OF ANCHORAGE STANDARD SPECIFICATIONS (M.A.S.S.), LATEST EDITION. REPLACE "AWWU" WITH "CITY OF KENAI" THROUGHOUT SPECIFICATIONS. PROJECT DRAWINGS AND PROJECT SPECIFICATIONS TAKE PRECEDENCE OVER M.A.S.S.
- CONTRACTOR IS REQUIRED TO FIND ALL PROPERTY PINS/MONUMENTS WITHIN PROJECT AREA AND PROTECT OR REPLACE AFTER CONSTRUCTION IS COMPLETE.
- SEED ALL AREAS DISTURBED BY CONSTRUCTION PER M.A.S.S., LATEST EDITION.
- CONTOURS SHOWN ON THE PLANS ARE FOR INFORMATIONAL PURPOSES AND SHOULD ONLY BE USED AS A REPRESENTATION OF EXISTING GRADES. SURVEYED ELEVATIONS ARE SHOWN ON THE ROAD PROFILES.
- CONTRACTOR SHALL ADHERE TO LOCAL REQUIREMENTS FOR NOISE, HOURS OF OPERATION, AND DUST CONTROL.
- TRAFFIC MARKINGS:
-PROVIDE 4" DOUBLE SOLID YELLOW STRIP AT CENTERLINE OF ROAD
-PROVIDE 4" SOLID WHITE FOGLINE AT SHOULDERS

DRIVEWAY AND CULVERT SCHEDULE

ITEM	STATION (FEET)	SIDE	WIDTH (FEET)	RADIUS (FEET)	NOTE	CMP/CPEP		
						REMOVE(FT)	LGTH (FT)	DIA. (IN.)
PUBLIC APPROACH	0+56.07	L	35	MATCH EXISTING	CURBS TO REMAIN	---	---	---
PUBLIC APPROACH	0+66.93	R	32	MATCH EXISTING	CONCRETE TO REMAIN	---	---	---
PUBLIC APPROACH	7+63.11	R	36	MATCH EXISTING	CONCRETE TO REMAIN	---	---	---
PUBLIC APPROACH	7+68.40	L	24	MATCH EXISTING	CURBS TO REMAIN	---	---	---
DW-1	9+10.00	L	42	MATCH EXISTING	---	---	---	---
PUBLIC APPROACH	10+52.75	L	24	MATCH EXISTING	CURBS TO REMAIN	---	---	---
PUBLIC APPROACH	10+56.59	R	36	MATCH EXISTING	CURBS TO REMAIN	---	---	---
DW-2	11+97.17	L	34	20	PER DETAIL B/C8	---	58	15
DW-3	13+46.26	L	42	20	PER DETAIL B/C8	---	64	15
DW-4	13+63.84	R	31	20	PER DETAIL B/C8	---	53	15
CROSS CULVERT	14+00.00	-	---	---	---	---	38	18
DW-5	16+88.12	R	24	20	PER DETAIL B/C8	---	46	15
DW-6	17+44.63	R	21	20	PER DETAIL B/C8	---	43	15
DW-7	18+12.16	L	15	20	PER DETAIL B/C8	---	37	15
DW-8	18+73.24	R	33	20	PER DETAIL B/C8	---	55	15
DW-9	20+54.03	L	33	20	PER DETAIL B/C8	---	55	15
DW-10	21+41.25	R	24	20	PER DETAIL B/C8	---	46	15
DW-11	21+93.31	L	32	20	PER DETAIL B/C8	---	54	15
DW-12	23+90.98	R	25	20	PER DETAIL B/C8	49	47	15
DW-13	24+46.55	L	24	20	PER DETAIL B/C8	---	46	15
DW-14	25+54.51	R	24	20	PER DETAIL B/C8	46	46	15
PUBLIC APPROACH	25+60.35	L	26	20	PER DETAIL B/C8	---	48	18
CROSS CULVERT	26+25.00	-	---	---	---	38	38	18
DW-15	26+82.21	L	21	20	PER DETAIL B/C8	---	43	15
DW-16	29+27.15	L	12	5X5 TAPER	PER DETAIL A/C8	40	34	15
DW-17	29+50.03	R	26	20	PER DETAIL B/C8	40	48	15
DW-18	29+82.99	L	26	20	PER DETAIL B/C8	55	48	15
DW-19	31+98.11	L	26	20	PER DETAIL B/C8	57	48	15
DW-20	35+56.04	L	24	20	PER DETAIL B/C8	60	50	15
CROSS CULVERT	35+96.65	-	---	---	---	40	38	18

NOTE: DRIVEWAY WIDTH AND RADIUS, AND CULVERT LENGTHS AND INVERTS MAY BE MODIFIED BY THE ENGINEER AS REQUIRED, TO FIT FIELD CONDITIONS. ALL GRADING AND DITCHING NECESSARY TO DIRECT WATER INTO OR OUT OF THE CULVERT, ARE INCIDENTAL ITEMS AND NO SEPARATE PAYMENT SHALL BE MADE.

CONTROL POINTS

POINT	DESCRIPTION	NORTHING	EASTING	ELEVATION
3	1.5" ALUMINUM MONUMENT	2400136.12	1416348.96	98.14
4	REBAR WITH YELLOW PLASTIC CAP	2400818.37	1416642.68	98.36
5	REBAR WITH RED PLASTIC CAP	2402652.21	1417807.79	100.61
6	2" ALUMINUM MONUMENT	2402248.95	1417560.79	99.59
TBM 7	NORTH BOLT ON UPPER HYDRANT FLANGE	2402587	1417867	102.56
TBM 8	NORTH BOLT ON UPPER HYDRANT FLANGE	2399649	1415958	98.98

SSMH/VB/MON ADJUSTMENT SCHEDULE

ITEM	STATION	OFFSET	EXISTING ELEV	PROPOSED ELEV
MONUMENT	10+62.93	---	96.17	96.23
GATE VALVE	25+51.18	31.70L	97.47	97.52
GATE VALVE	26+99.75	5.62R	97.34	98.11
SSMH	27+10.84	7.85L	97.60	98.12
SSMH	29+27.01	4.88L	98.41	99.15
GATE VALVE	29+97.43	4.04L	98.57	99.49
SSMH	32+73.63	8.11L	99.69	100.65
SSMH	35+84.30	7.79L	99.24	99.29

DRAINAGE GALLERY SCHEDULE

START STATION	END STATION	SIDE	LENGTH
14+00.00	14+50.00	LEFT/RIGHT	50-FT/50-FT
26+00.00	26+50.00	LEFT/RIGHT	50-FT/50-FT
36+00.00	36+50.00	LEFT/RIGHT	50-FT/50-FT



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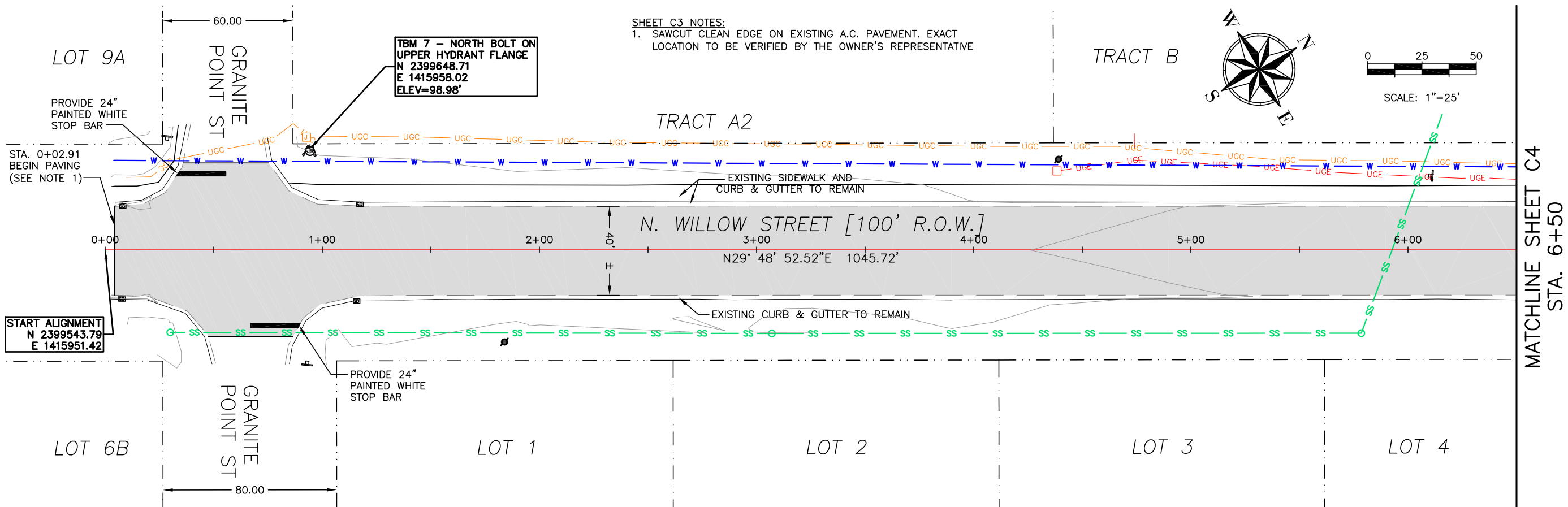
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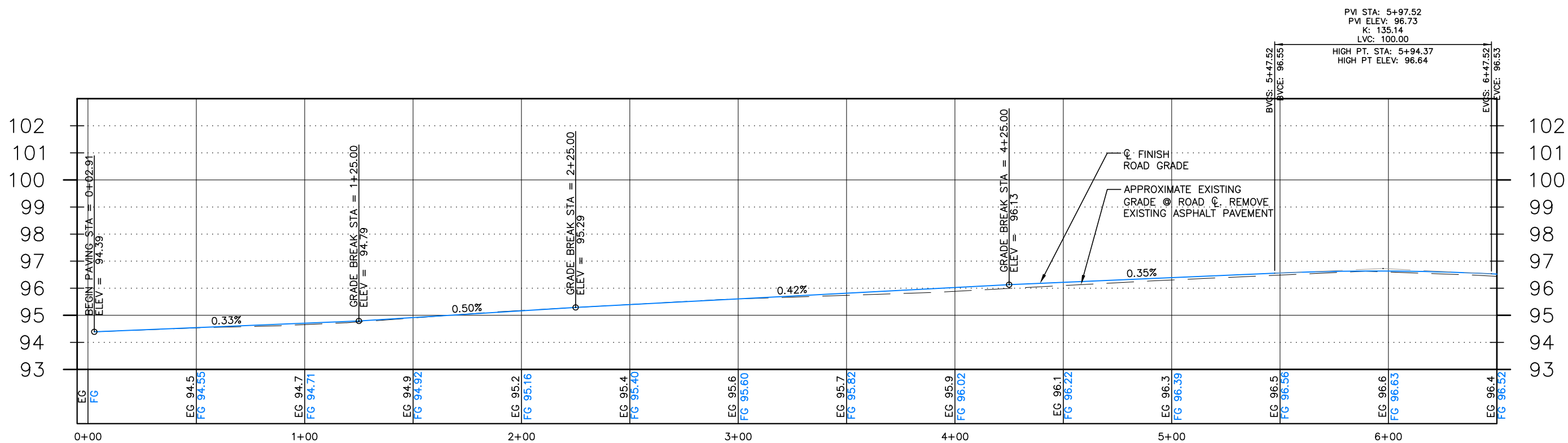
NELSON
ENGINEERING

2023 N. WILLOW ST. ROADWAY IMPROVEMENTS
CITY OF KENAI
KENAI, ALASKA
NOTES, LEGEND, ABBREVIATIONS

PROJECT NO.
2022030.2
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CHECKED BY:
MJD/MZD
DATE: 5/14/2023
SCALES: NOTED
HORIZ.: NOTED
VERT.: NOTED
SHEET: **C2**



A
C3 N. WILLOW STREET - PLAN (STA. 0+00 TO 6+50)
HORIZONTAL SCALE: 1" = 25' (22X34), 1" = 50' (11X17)



B
C3 N. WILLOW STREET - CL ROAD PROFILE (STA. 0+00 TO 6+50)
HORIZONTAL SCALE: 1" = 25' (22X34), 1" = 50' (11X17); VERTICAL SCALE: 1" = 2' (22X34), 1" = 4' (11X17)



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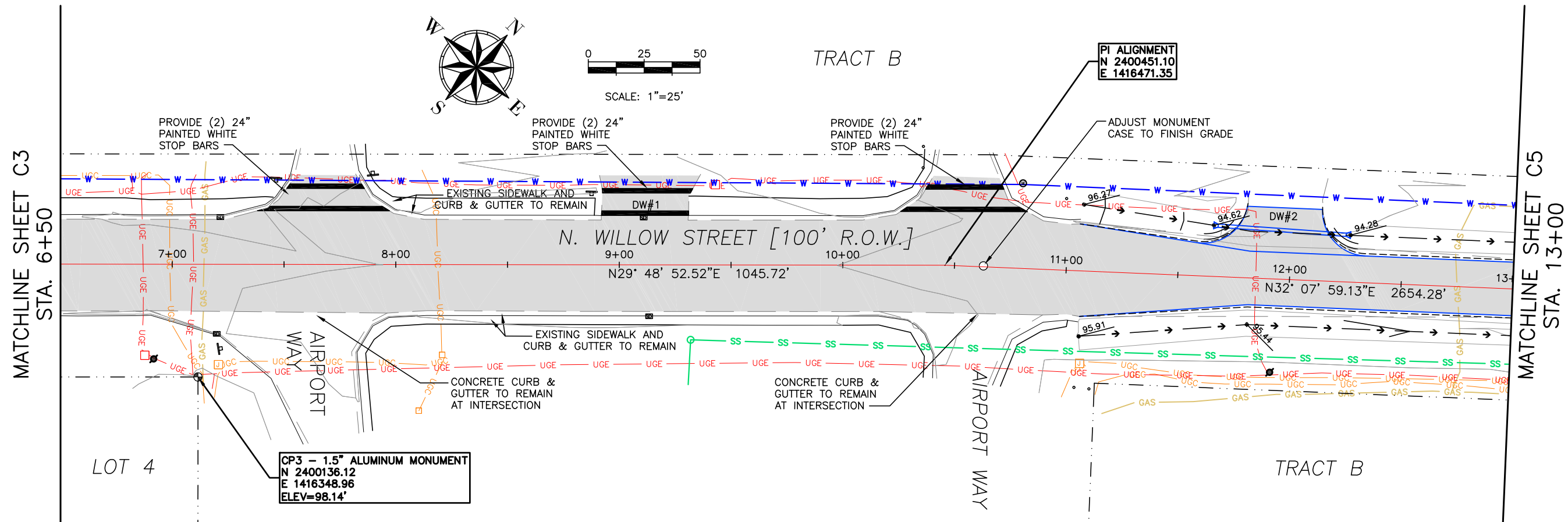
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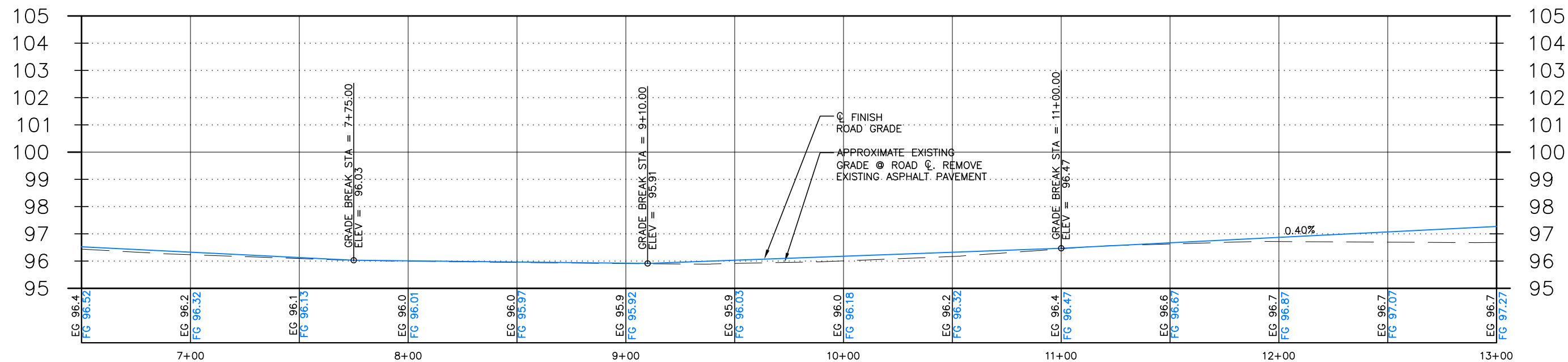
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2023 N. WILLOW ST. ROADWAY IMPROVEMENTS
CITY OF KENAI
PLAN AND PROFILE
STA. 0+00 TO 6+50

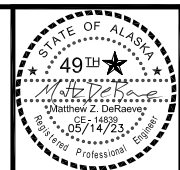
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DATE: 5/14/2023
SCALES: NOTED
HORIZ. NOTED
VERT. NOTED
SHEET: **C3**
3 OF 13



A **N. WILLOW STREET - PLAN (STA. 6+50 TO 13+00)**
C4 HORIZONTAL SCALE: 1" = 25' (22X34), 1" = 50' (11X17)



B **N. WILLOW STREET - CL ROAD PROFILE (STA. 6+50 TO 13+00)**
C4 HORIZONTAL SCALE: 1" = 25' (22X34), 1" = 50' (11X17); VERTICAL SCALE: 1" = 2' (22X34), 1" = 4' (11X17)



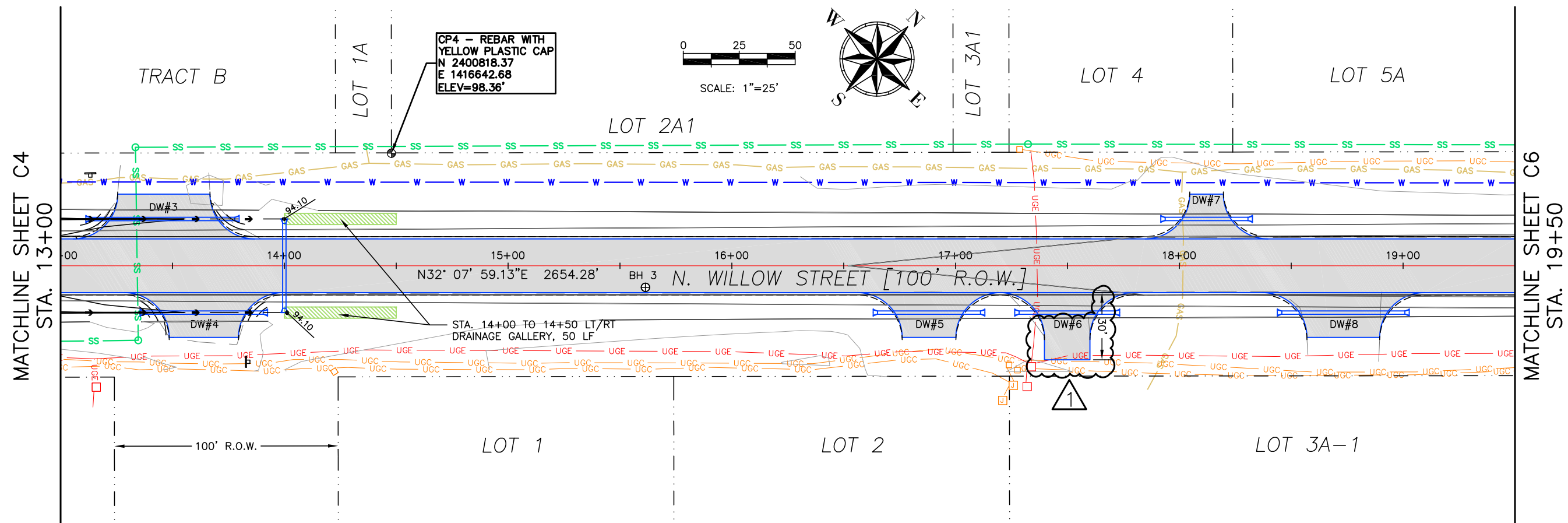
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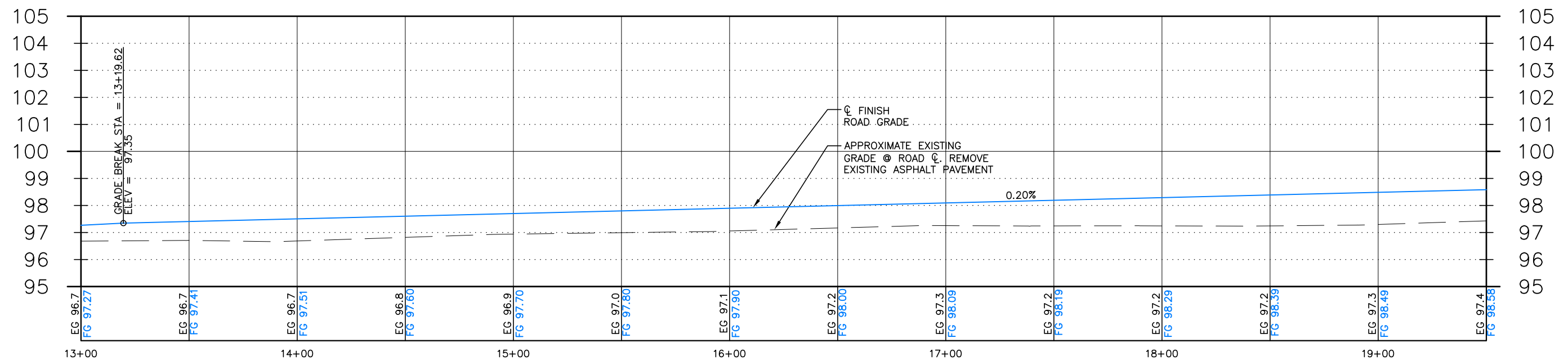
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PLAN AND PROFILE
STA. 6+50 TO 13+00

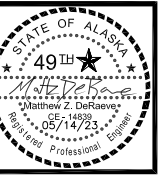
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 HORIZ. NOTED
 VERT. NOTED
 SHEET:
C4
 4 OF 13



A N. WILLOW STREET - PLAN (STA. 13+00 TO 19+50)
C5 HORIZONTAL SCALE: 1" = 25' (22X34), 1" = 50' (11X17)



B N. WILLOW STREET - CL ROAD PROFILE (STA. 13+00 TO 19+50)
C5 HORIZONTAL SCALE: 1" = 25' (22X34), 1" = 50' (11X17); VERTICAL SCALE: 1" = 2' (22X34), 1" = 4' (11X17)



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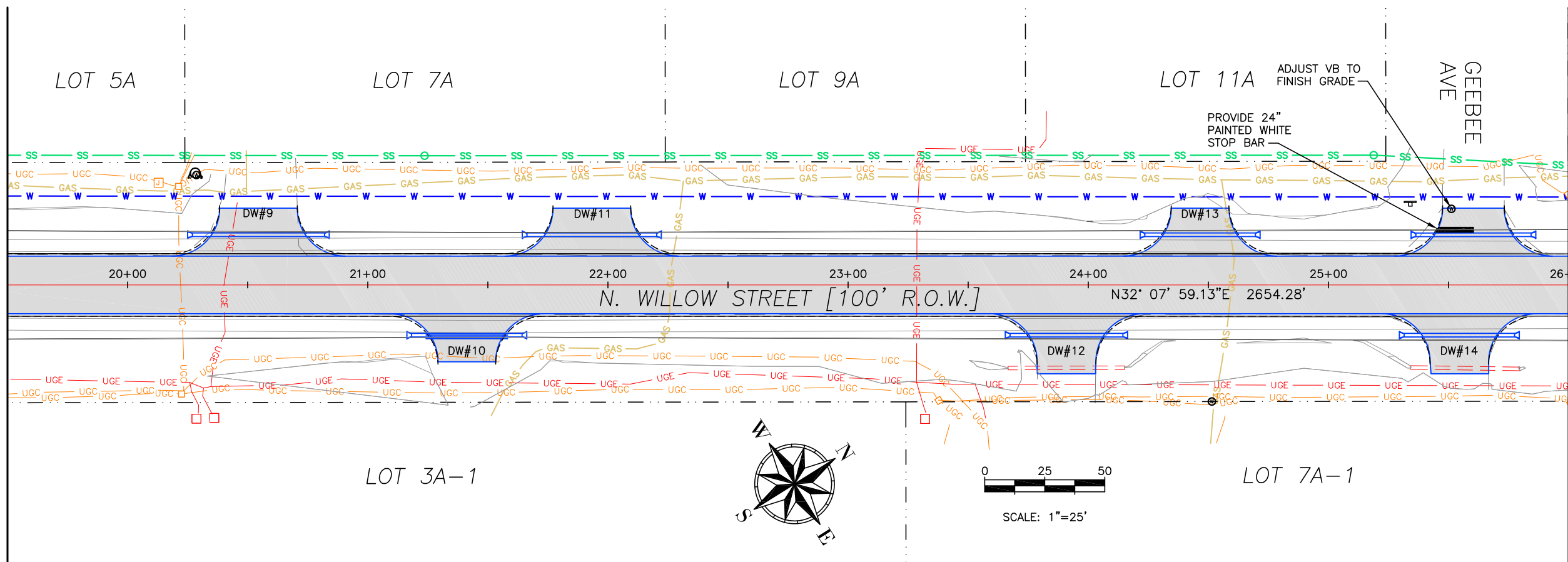
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ENGINEERING

2023 N. WILLOW ST. ROADWAY IMPROVEMENTS
CITY OF KENAI
 PLAN AND PROFILE
 STA. 13+00 TO 19+50

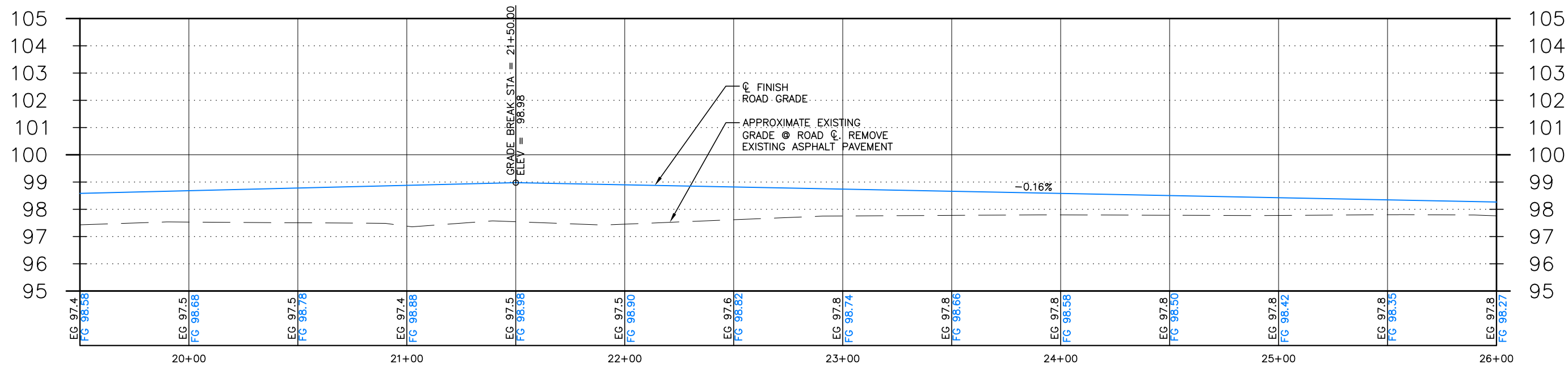
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 HORIZ.: NOTED
 VERT.: NOTED
 SHEET: **C5**

MATCHLINE SHEET C5
STA. 19+50



MATCHLINE SHEET C7
STA. 26+00

A
C6 N. WILLOW STREET - PLAN (STA. 19+50 TO 26+00)
HORIZONTAL SCALE: 1" = 25' (22X34), 1" = 50' (11X17)



1 REVISED ROAD PROFILE

B
C6 N. WILLOW STREET - CL ROAD PROFILE (STA. 19+50 TO 26+00)
HORIZONTAL SCALE: 1" = 25' (22X34), 1" = 50' (11X17); VERTICAL SCALE: 1" = 2' (22X34), 1" = 4' (11X17)



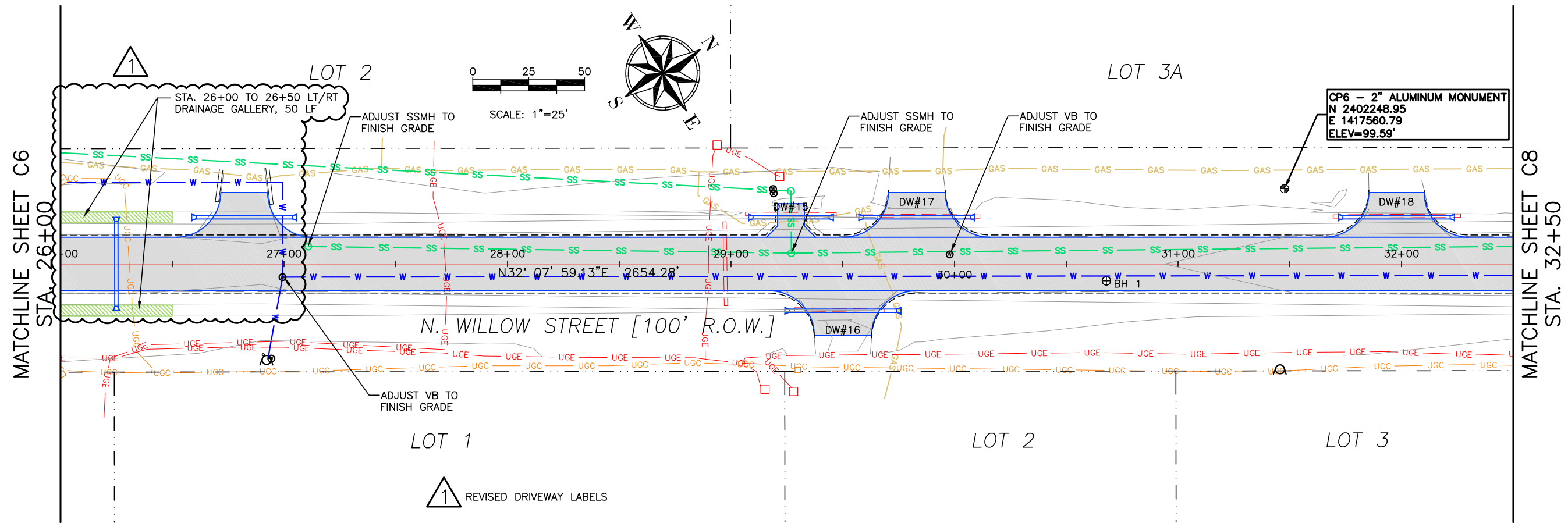
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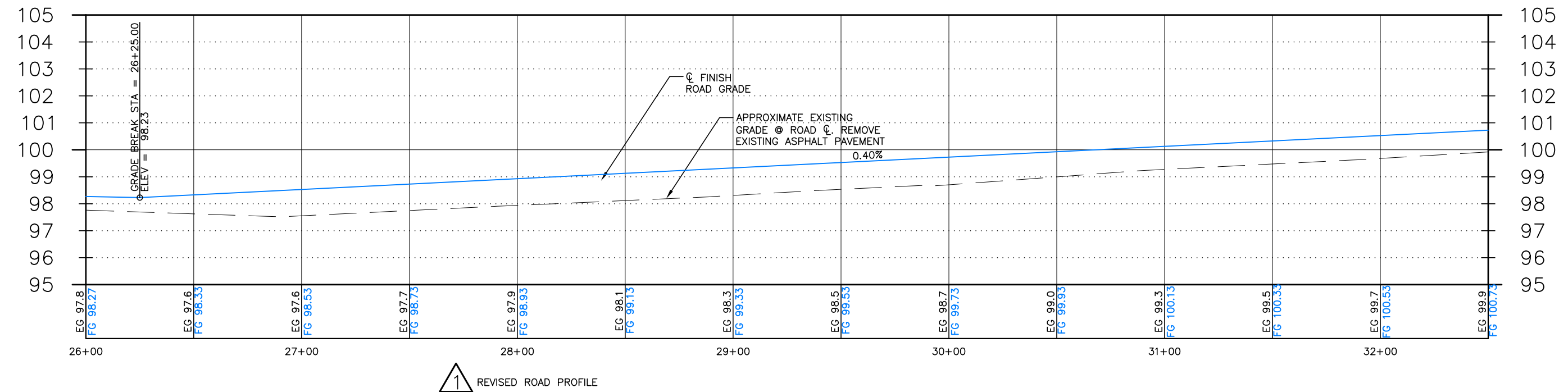
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CITY OF KENAI
PLAN AND PROFILE
STA. 19+50 TO 26+00

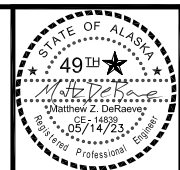
PROJECT NO.
2022030.2
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MJD/MZD
DATE: 5/14/2023
SCALES: NOTED
HORIZ. NOTED
VERT. NOTED
SHEET: **C6**
6 OF 13



A N. WILLOW STREET - PLAN (STA. 26+00 TO 32+50)
C7 HORIZONTAL SCALE: 1" = 25' (22X34), 1" = 50' (11X17)



B N. WILLOW STREET - CL ROAD PROFILE (STA. 26+00 TO 32+50)
C7 HORIZONTAL SCALE: 1" = 25' (22X34), 1" = 50' (11X17); VERTICAL SCALE: 1" = 2' (22X34), 1" = 4' (11X17)



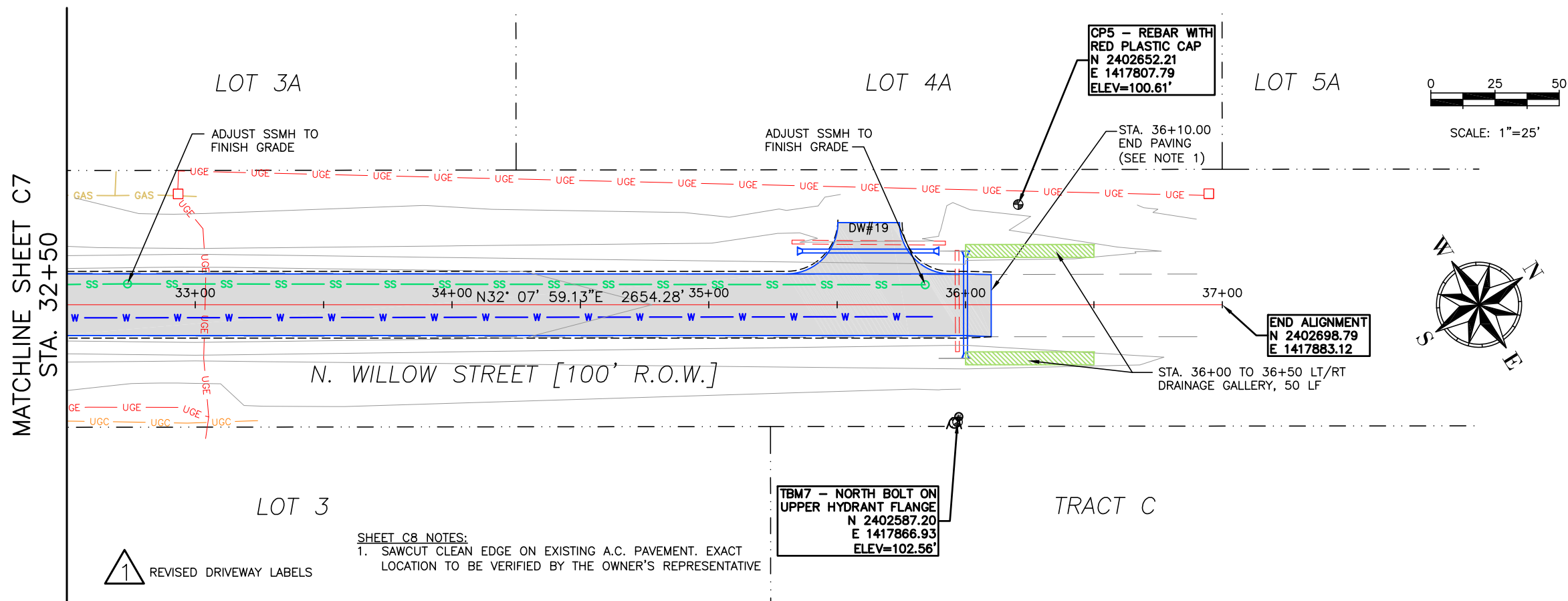
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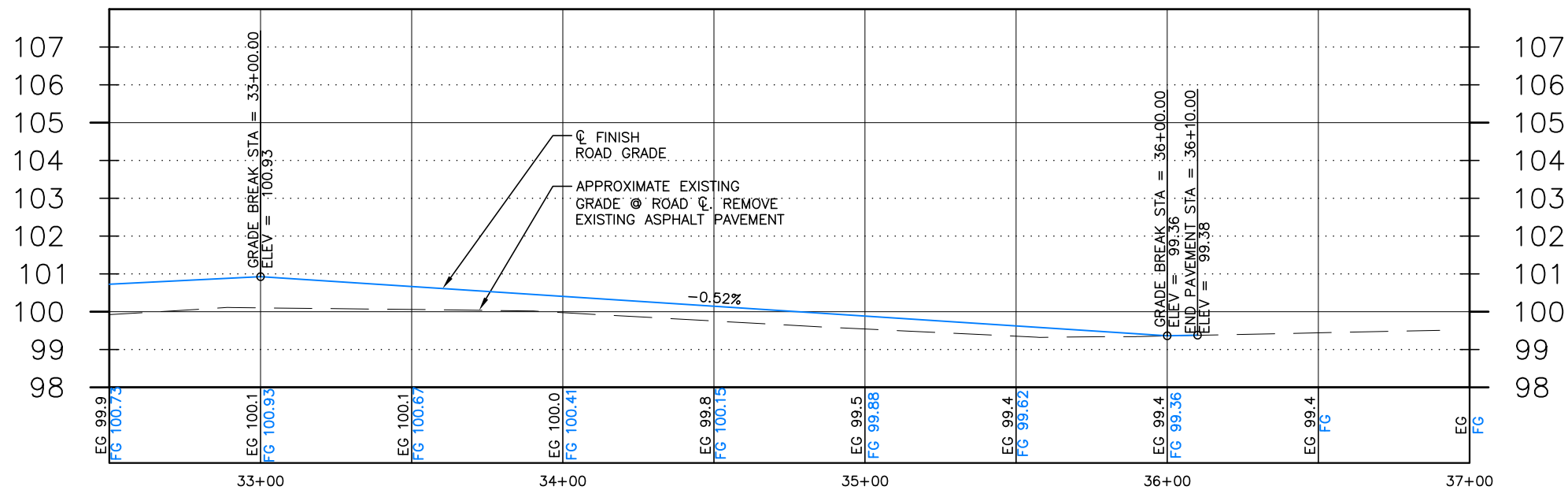
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STA. 26+00 TO 32+50

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SHEET: **C7**
7 OF 13



A
C8 **N. WILLOW STREET - PLAN (STA. 32+50 TO 37+00)**
HORIZONTAL SCALE: 1" = 25' (22X34), 1" = 50' (11X17)



B
C8 **N. WILLOW STREET - CL ROAD PROFILE (STA. 32+50 TO 37+00)**
HORIZONTAL SCALE: 1" = 25' (22X34), 1" = 50' (11X17); VERTICAL SCALE: 1" = 2' (22X34), 1" = 4' (11X17)



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8 OF 13



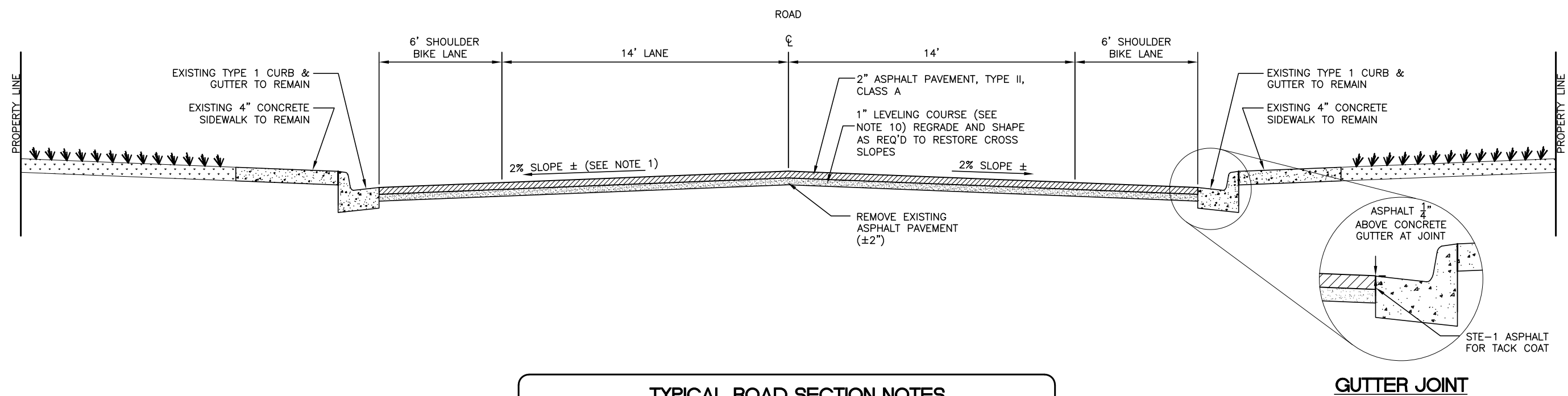
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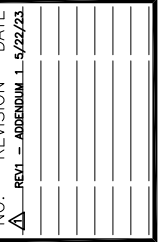
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KENAI, ALASKA
TYPICAL ROAD SECTIONS

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9 OF 13



- TYPICAL ROAD SECTION NOTES**
1. SAW CUT CLEAN EDGES ON EXISTING ASPHALT AT CONNECTING STREETS AND DRIVEWAYS PRIOR TO REMOVING THE ASPHALT. DISPOSE OF EXISTING ASPHALT PAVEMENT AT CONTRACTORS'S APPROVED DISPOSAL SITE.
 2. REGRADE AND RESHAPE EXISTING BASE COURSE AS REQUIRED. IMPORT ADDITIONAL BASE COURSE AND COMPACT PER SPECIFICATIONS. SUBBASE RECONSTRUCTION MAY BE REQUIRED IF UNSUITABLE SUBGRADE CONDITIONS ARE DISCOVERED. ADDITIONAL EXCAVATION MAY BE REQUIRED AS DIRECTED BY THE ENGINEER.
 3. OBTAIN APPROVAL FOR BASE AND SUBBASE BY ENGINEER.
 4. MATCH EXISTING CROSS SLOPES OF THE ROADWAY UNLESS NOTED OTHERWISE.
 5. MATCH EXISTING PAVEMENT ELEVATION AT APPROACHES AND CONNECTING STREETS. COORDINATE EXACT LOCATION OF THE TIE-IN WITH THE OWNER'S REPRESENTATIVE.
 6. APPLY TACK COAT TO FACE OF ASPHALT EDGE WHERE MATCHING EXISTING ASPHALT PAVEMENT AND AS SPECIFIED BY THE ENGINEER.
 7. TRANSITION CROSS SLOPE TO MATCH EXISTING AT THE EDGE OF PAVEMENT OF THE SIDE STREETS/DRIVEWAYS AND AT THE BOP AND EOP A MINIMUM OF 25 FEET.
 8. UNDERGROUND UTILITIES ARE PRESENT WITHIN THE PROJECT AREA. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING LOCATES, WORKING AROUND UTILITIES, AND COORDINATING WITH UTILITY COMPANIES AS REQUIRED.



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TYPICAL ROAD SECTIONS

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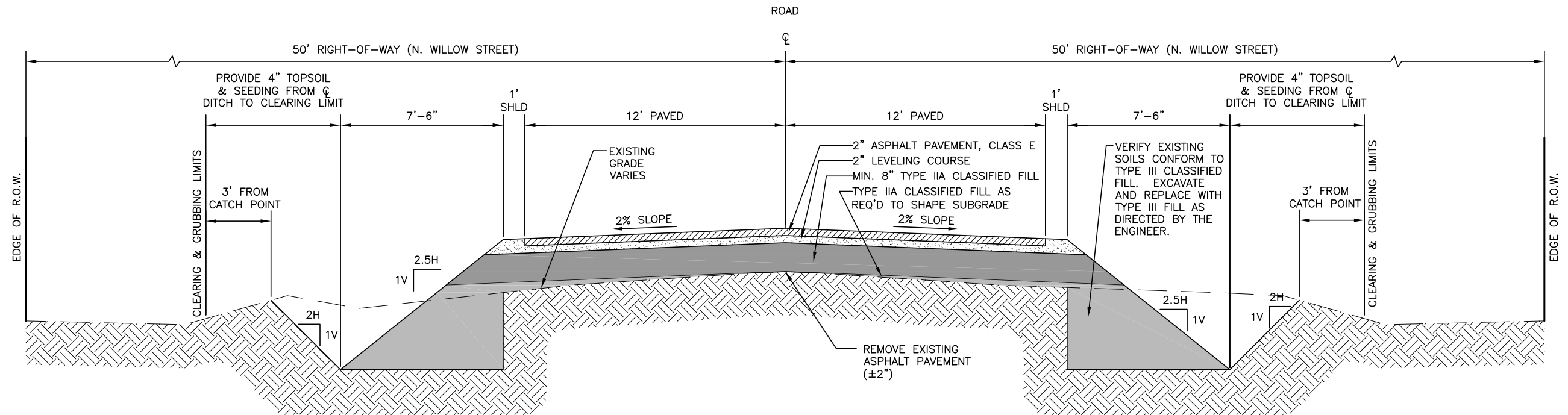
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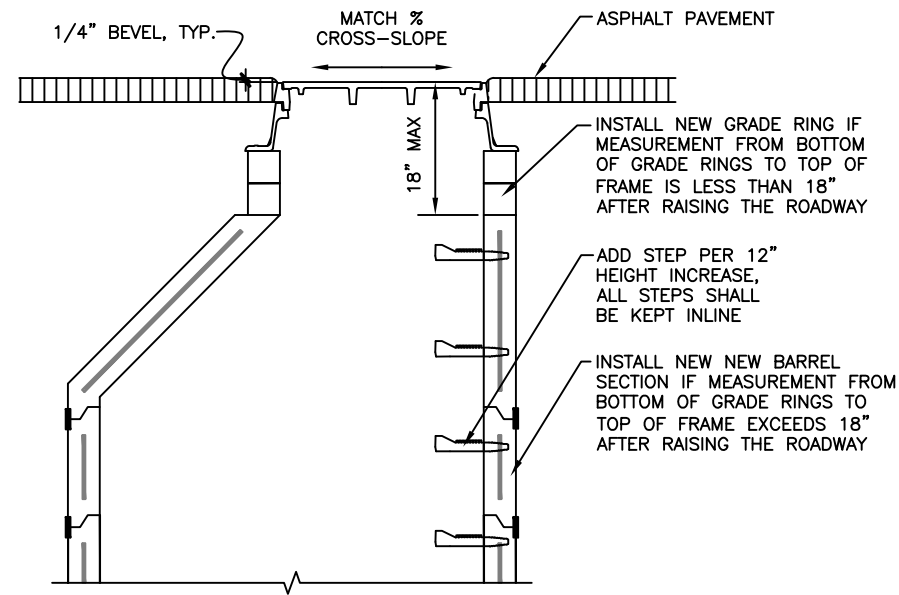
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SHEET: C10

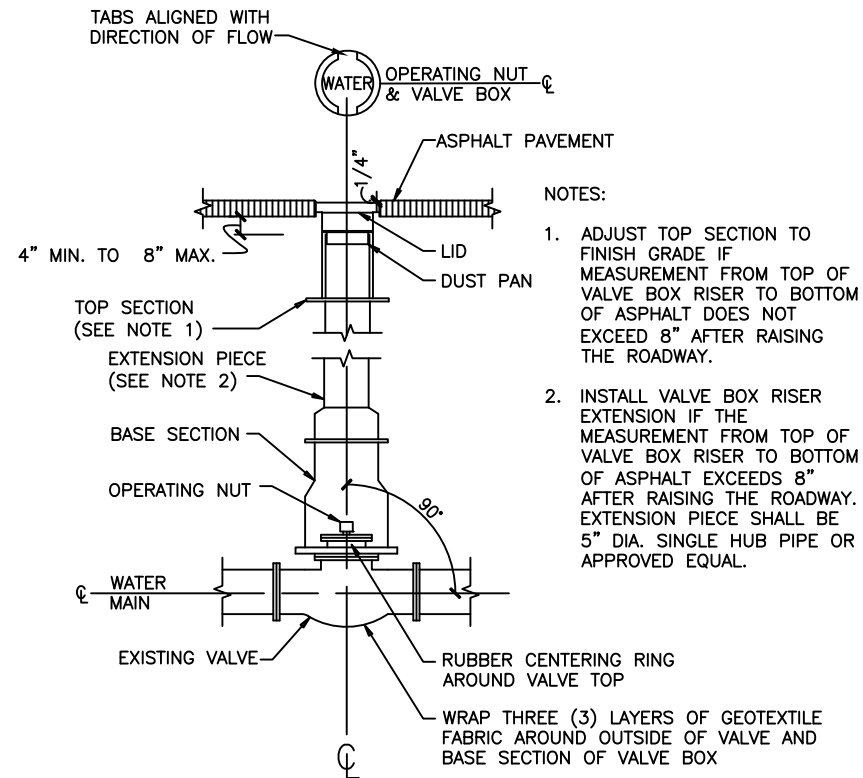
10 OF 13



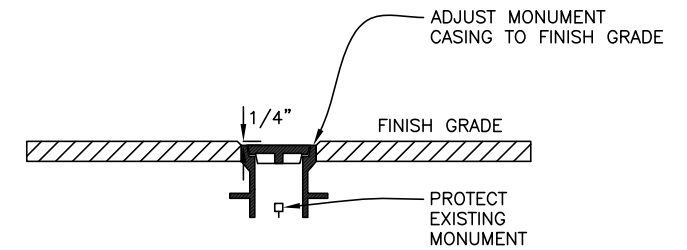
1. SAW CUT CLEAN EDGES ON EXISTING ASPHALT AT CONNECTING STREETS AND DRIVEWAYS PRIOR TO REMOVING THE ASPHALT. REMOVE ALL ASPHALT WITHIN LIMITS OF AREA SHOWN ON PLANS. DISPOSE OF EXISTING ASPHALT PAVEMENT AT CONTRACTOR'S APPROVED DISPOSAL SITE.
2. REGRADE, RESHAPE, AND SCARIFY EXISTING BASE/SUBBASE AS REQUIRED TO RECEIVE NEW FILL. MATERIAL REMOVED FROM THE HIGH AREAS SHALL BE USED TO FILL THE DEPRESSIONS AND SHOULDER WIDENING AREAS. SUBBASE RECONSTRUCTION MAY BE REQUIRED IF UNSUITABLE SUBGRADE CONDITIONS ARE DISCOVERED. ADDITIONAL EXCAVATION MAY BE REQUIRED AS DIRECTED BY THE ENGINEER.
3. OBTAIN APPROVAL FOR BASE AND SUBBASE BY ENGINEER.
4. PLACE ALL FILL IN LOOSE LIFTS NOT EXCEEDING TWELVE INCHES (12") AND MECHANICALLY COMPACT EACH LAYER TO NOT LESS THAN NINETY-FIVE PERCENT (95%) OF THE MAXIMUM DENSITY AT OPTIMUM MOISTURE, UNLESS OTHERWISE NOTED AND APPROVED BY THE ENGINEER.
5. MATCH EXISTING PAVEMENT ELEVATION AT APPROACHES AND CONNECTING STREETS. COORDINATE EXACT LOCATION OF THE TIE-IN WITH THE OWNER'S REPRESENTATIVE.
6. APPLY TACK COAT TO FACE OF ASPHALT EDGE WHERE MATCHING EXISTING ASPHALT PAVEMENT AND AS SPECIFIED BY THE ENGINEER.
7. TRANSITION CROSS SLOPE TO MATCH EXISTING AT THE EDGE OF PAVEMENT OF THE SIDE STREETS/DRIVEWAYS AND AT THE BOP AND EOP A MINIMUM OF 25 FEET.
8. UNDERGROUND UTILITIES ARE PRESENT WITHIN THE PROJECT AREA. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING LOCATES, WORKING AROUND UTILITIES, AND COORDINATING WITH UTILITY COMPANIES AS REQUIRED.



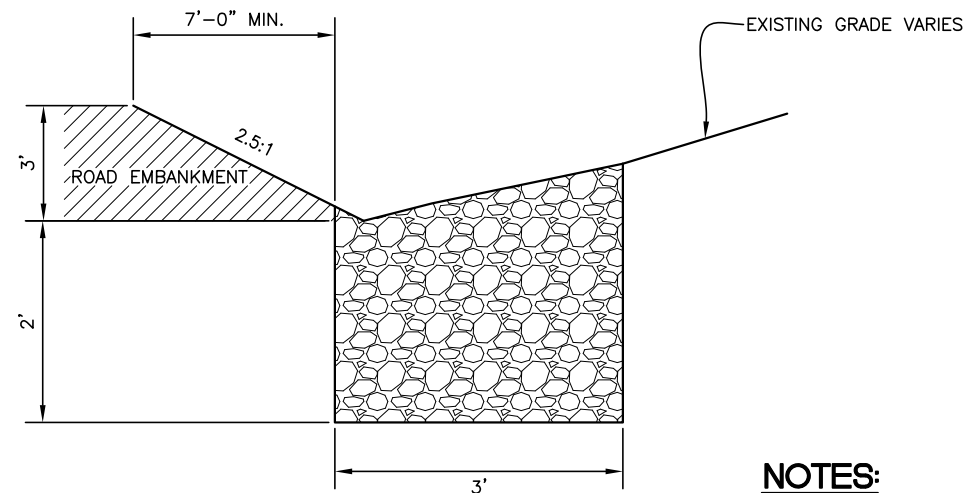
A MANHOLE ADJUSTMENT DETAIL
C11 SCALE: NTS



B TYPICAL VALVE BOX ADJUSTMENT
C11 SCALE: NTS



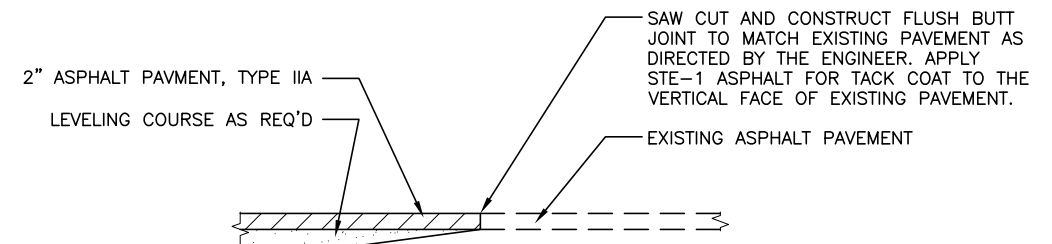
C TYP. MONUMENT CASING ADJUSTMENT
C11 SCALE: NTS



NOTES:

1. DRAIN ROCK SHALL REMAIN FREE OF SILT AND OTHER UNDESIRABLE MATERIALS DURING PLACEMENT.
2. DO NOT PLACE ROCK MORE THAN 4" ABOVE BOTTOM OF DITCH.
3. LOCATION AND POSITION OF DRAINAGE GALLERIES MAY BE CHANGED BY THE ENGINEER TO AVOID CONFLICTS WITH UTILITIES AND OTHER STRUCTURES.

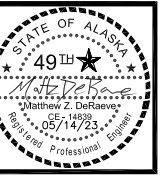
D TYPICAL DRAINAGE GALLERY
C11 SCALE: NTS



NOTES:

1. MATCH EXISTING PAVEMENT ELEVATION AT CONNECTING STREETS
2. USE TACK COAT WHERE MATCHING EXISTING ASPHALT OR CONCRETE AND AS SPECIFIED BY THE ENGINEER.

E TYPICAL PAVEMENT MATCH DETAIL
C11 SCALE: NTS



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TYPICAL DETAILS

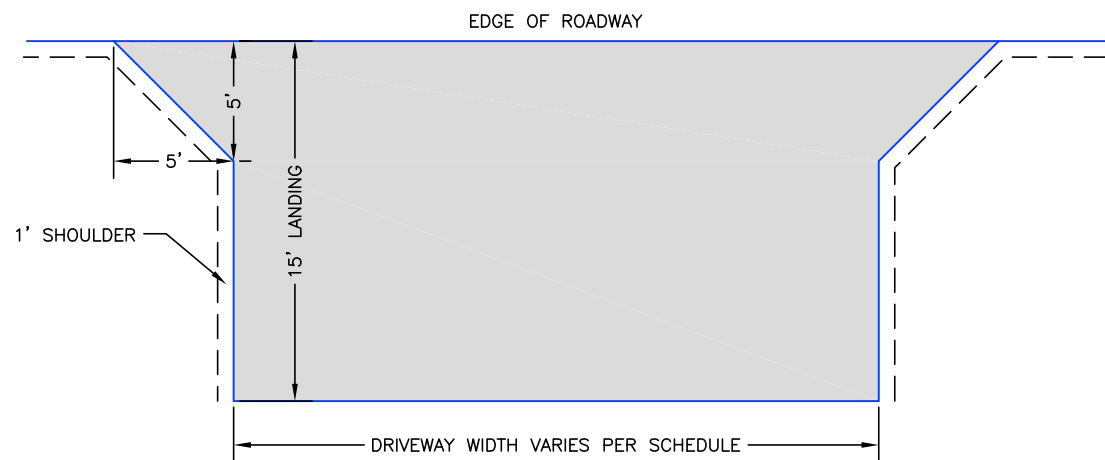
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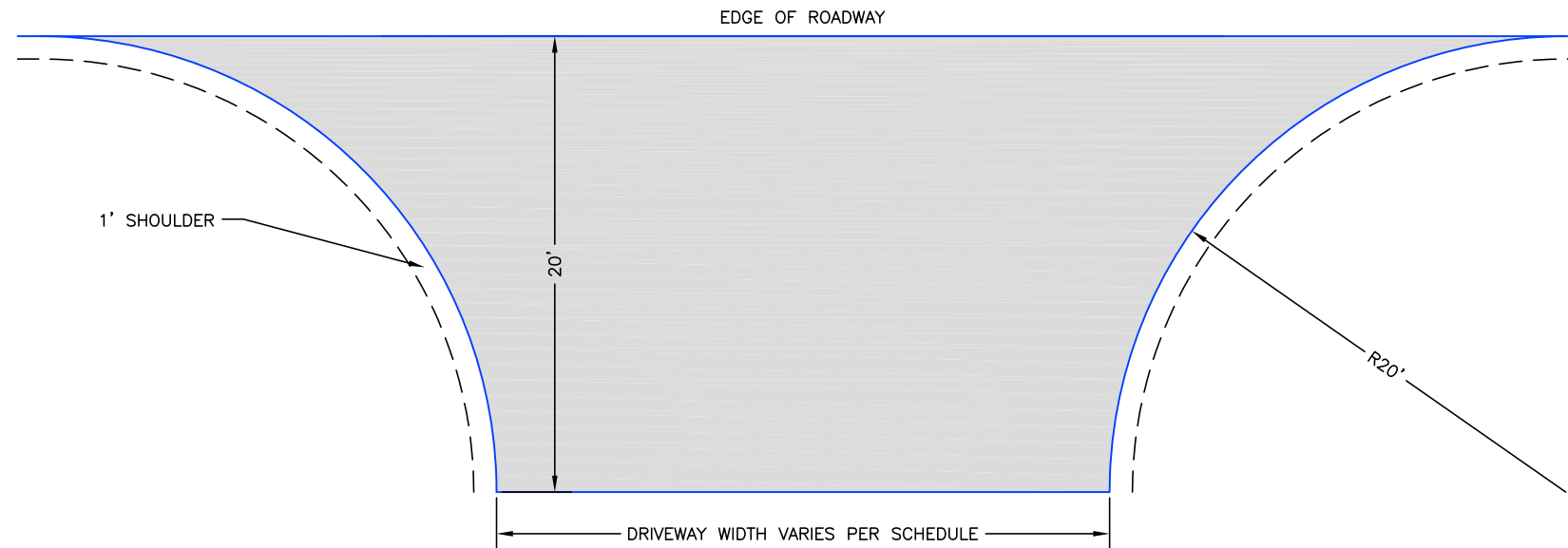
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HORIZ.: NOTED
VERT.: NOTED
SHEET: **C11**



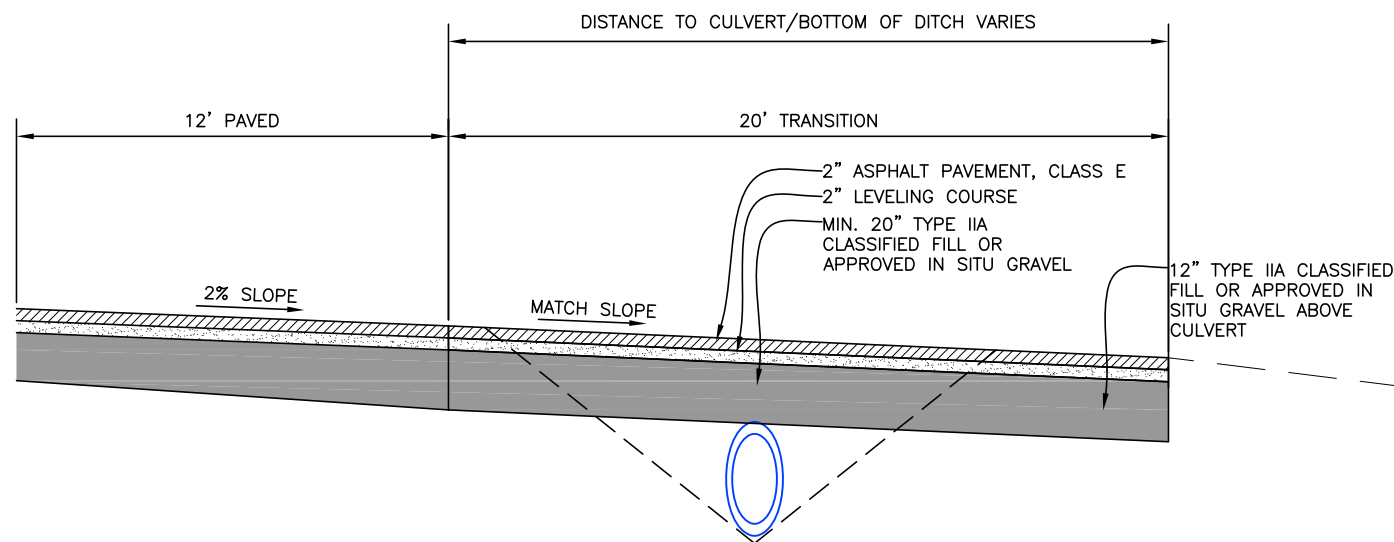
PLAN VIEW

A TYPICAL DRIVEWAY - RESIDENTIAL/HYDRANT PAD
C12 SCALE: NTS



PLAN VIEW

B TYPICAL DRIVEWAY - COMMERCIAL
C12 SCALE: NTS



SECTION VIEW

C TYPICAL DRIVEWAY LANDING
C12 SCALE: NTS



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12 OF 13



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13 OF 13

